

Town of Lisbon, Maine

Route 196 Corridor Vision & Master Plan



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For More Information

Have questions, or want to know more about something in the Plan? Please contact the Lisbon Town Office at 207-353-3000 or see www.lisbonme.org. A project website link for the Route 196 Master Plan can also be found on the Town of Lisbon webpage, under the Planning Board page, which contains past presentations and information from the plan's development.

Abbreviations and References

ADA (accessibility) Americans with Disabilities Act

Refers to the set of standards for universal accessibility for persons of all abilities, as set forth in the Act.

ATRC Androscoggin Transportation Resource Center

Refers to the regional transportation agency for Lisbon, a source of planning assistance, data and coordination with MaineDOT, and potential state and federal transportation funding.

CIP Capital Improvements Plan

Refers to a municipality's overall plan for significant capital investments/expenses, for short- or intermediate-term budgeting.

MaineDOT Maine Department of Transportation

The state's transportation authority, which has jurisdiction over state route 196 and is the primary source for funding for transportation-related planning and improvements.

TIF Tax Increment Financing

Refers to a type of local tax program that "captures" the increase (or increment) in taxes from new development or property improvements, within a predetermined district.

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Executive Summary

Overview

This Route 196 Master Plan represents the collective efforts of numerous, dedicated community members with an interest in improving prosperity and quality of life in Lisbon. The process featured three community forums where members of the public discussed the Route 196 Corridor in three segments, and featured the development of an informal project working group, the Steering Committee, which met on a regular basis. Joint meetings between the Steering Committee, Council, Planning Board, and School Facilities Committee were also held in order to refine priorities and discuss how the Plan will be used moving forward.

This Plan provides a clearer community vision of the future of the Route 196 Corridor, building on the Town's Comprehensive Plan, and identifies what is important to shaping a more positive and prosperous future. It outlines the challenges and opportunities along different areas of the corridor, from village to commercial highway. And section by section, the Plan identifies the community's priorities for improving the future of Route 196.

As noted in the Introduction, Lisbon's image and character is identified as critical to community and economic development. This issue is the major focus of the Route 196 Master Plan, looking at the community's values for and the impact of land use, design, public and private improvements, and redevelopment opportunities along the corridor.

The Route 196 Plan touches upon many other important planning elements, but does not yet represent a comprehensive assessment and recommendations on all of these. This includes traffic and transportation, utilities and infrastructure, and conservation and recreation. All of these elements do have very important roles in the future of Route 196 and growth in Lisbon, and must continue to be integrated into this Plan and vision in the coming years.

Moving the Plan Forward

It is important that a community planning process such as this results in some immediate and positive progress, maintaining the community's energy and interest in effecting change.

Over the next 1 to 2 years, the Town's goals for the development and implementation of the Route 196 Master Plan will be:

- Select and implement a few projects that will give the Route 196 planning effort visibility;
- Identify "low hanging fruit", projects or initiatives that don't take as much time/effort or funding and can build momentum and early success;
- Integrate the vision and Route 196 plan with economic development initiatives;
- Spread the word: reach out to individual business/property owners, stakeholders, community groups, and residents to build awareness of the vision, and look for opportunities to build new partnerships and implement elements of the plan;
- Identify likely funding opportunities, seek grant funding, reach out to local financial institutions, discuss TIF funding and other ways to provide town matching funds.



So Now What?

Summary Recommendations

After the completion of the community forums and Steering Committee meetings, a set of summary recommendations was developed to help guide the Town's next steps in improving the Route 196 Corridor. These reflect the community's vision and values, as well as technical and professional input and recommendations, with strategies that range from outreach, to policies/programs, to improvements and investments. Intermediate and long-term recommendations are also listed for some needs that were prominently discussed in this Plan.

Residential versus Commercial

In this Plan, the recommendations and strategies are intended to be applicable only to non-residential uses. While the *vision and planning* may be broadly inclusive of both residential and non-residential uses and structures, at this time the Plan does not include initiatives or improvements addressing residential uses along Route 196.

The Town may wish to consider developing a plan for addressing residential and housing needs on Route 196 in the long term.



Outreach

- Continue to collaborate with and encourage the work of Positive Change Lisbon.
- Engage immediately on "1-on-1" outreach to private business and property owners on Route 196, follow up on visioning and identify opportunities to support private improvements and listen to private owners' ideas and concerns.
- Engage immediately in outreach to appropriate stakeholders and community groups, to promote the Route 196 vision, increase awareness, and develop partnerships.
- Integrate and collaborate with the Lisbon Downtown Network on Route 196 efforts.



Policies, Programs, Planning

- Establish stronger design standards (regulations) for non-residential buildings and property along the Route 196 corridor, with design guidelines (non-regulatory) to support.
- Establish a program to provide design assistance to building owners willing to make façade/landscaping improvements, supported by revolving loans or small grants for implementation.
- Take proactive steps to redevelop the Worumbo Mill and waterfront area (option to include Knight-Celotex site as well).
- Develop a plan for public/private design improvements to the Midtown commercial area.





- Integrate the ATRC regional bike/ped plan into Route 196 efforts.
- Build a financial plan to support revitalization – TIF districts, CIP, identifying grant opportunities.
- Consider developing a plan addressing residential and housing issues along Route 196 in the long-term.

Improvements & Investments

- Begin phasing in new streetscape/decorative lampposts along Route 196 in the villages; add benches or other amenities where possible.
- Worumbo – consider purchasing property, clean up the site for reuse.
- Continue to work with and support the owners/developers of the Graziano's site and Knight-Celotex on plans for redevelopment – plan to invest in public streetscape/pedestrian improvements.
- Begin planning and phasing in implementation of pedestrian/bike improvements, including the proposed rail trail.

Intermediate/Long-term

- Work with MaineDOT to plan for future traffic and roadway needs between Canal Street and the High School/Route 9 intersection.
- Coordinate major public investments for Route 196 with the School System's long-term plans; the schools represent a significant portion of local budget and spending, and while both the schools' future plans and this Plan are important to the community, there must be forethought and coordination to avoid significant fiscal pressures and taxpayer burden.
- Develop further vision for future growth in areas with larger scale development potential, such as Kelly Park or the Diversified Development and Commercial Zones.
- Consider developing a Lisbon Bike/Pedestrian Plan to supplement the regional bike/pedestrian plan (by ATRC).
- Explore the feasibility of burying or relocating overhead wires, determine priority areas, explore phasing options.

Summary of Plan Priorities

(Next page.) The recommendations above were largely based on the identification of community priorities during the master planning process. The following is a summary of the corridor priorities, which were determined for each of three corridor segments and refined by the Steering Committee.



Priorities for Multiple Corridor Segments

Ideas or issues that were indicated as very important for two or all three of the corridor segments.

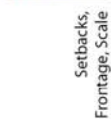
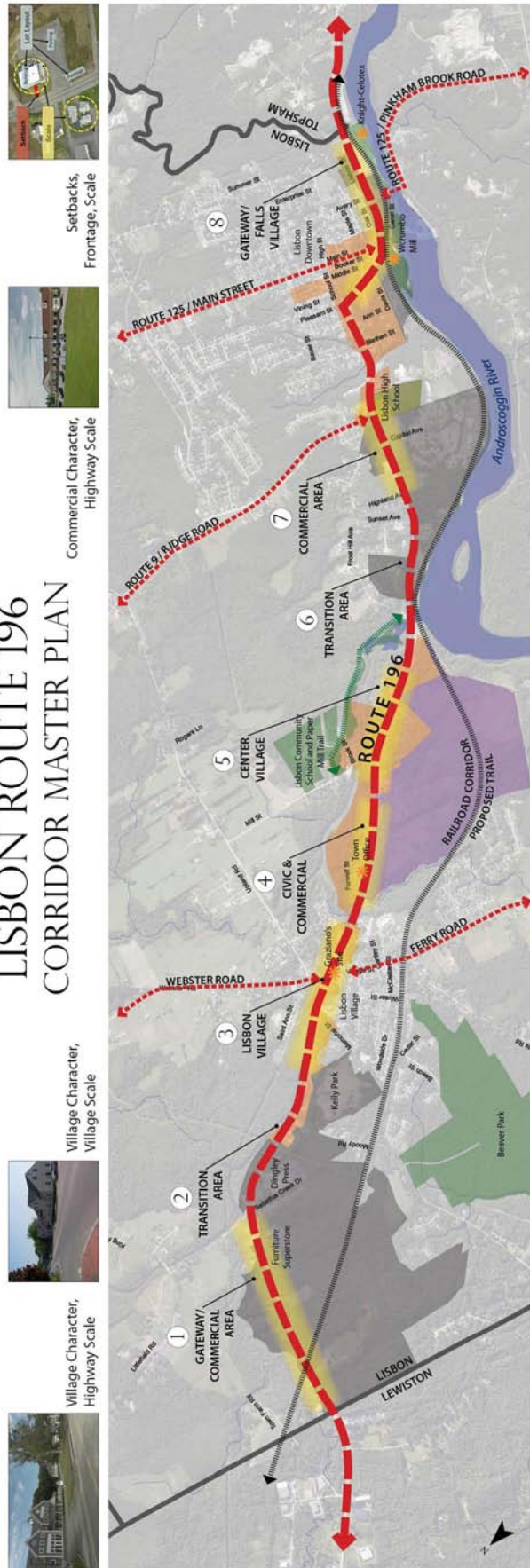
- Improve the town's gateways (Lewiston and Topsham ends of the corridor).
- Establish standards to address visual character and aesthetics for new development and redevelopment.
- Establish programs/incentives to address visual character for existing development, such as façade or site design assistance.
- Follow up on needed long-term transportation planning, addressing traffic capacity and road design throughout the corridor in a manner that compliments this plan.
- Assess the need for regulatory changes (land use and development), such as rezoning in the "transition areas" or considering changes to allowed land uses or dimensional standards.
- Pursue the establishment of a continuous multi-use rail trail (roughly paralleling Route 196), connecting Lisbon's villages and making regional connections to Lewiston and Topsham.
- Continue to improve pedestrian and bicycle conditions, such as sidewalks, crosswalks, bike lanes, and future rail trail connections.
- Make additional streetscape improvements, including street trees and landscaping, lampposts, banners, sidewalk design, and other amenities.
- Add new and enhance existing parks and green spaces proximal to the corridor (such as on the Lisbon Falls waterfront, or by the Flea Market/Lisbon Village).
- Support a long-term strategy for relocating overhead utilities underground.

Priorities for Individual Segments

Ideas or issues indicated as important for only one section of the corridor – though several are still applicable to the entire corridor.

- Ensure traffic mobility and safety continue to be addressed through standards and alternative traffic design, avoiding the need for traffic lights and adding lanes (road widening).
- Retain the scenic (river) and historic/cultural character north of Lisbon Village.
- Seek opportunities to develop public-private partnerships to revitalize the waterfront area, including the Worumbo Mill and Knight-Celotex sites.
- Maintain and enhance the existing wooded character of the Midtown Transition Area.
- Maintain the "fabric" of the Center Village, but allow for conversion from residential to non-residential.
- Support opportunities for residential growth off Mill Street.
- Take a more active role in visioning and planning for future growth in the Diversified Development Zone, its relationship to Route 196 and the village.
- Support opportunities for residential growth off Mill Street.
- Work with developers of the former Graziano's site to build positive village character at the Village Street intersection; invest in public improvements (streetscape, pedestrian) in this area.
- Work to revitalize/enhance the strip north of the Village Street intersection – façades, redevelopment, and streetscape.
- Evaluate options to improve safety and traffic speeds at the Moody Road intersection.
- Retain the scenic and historic/cultural character for the Northern Transition Area between Littlefield Road and Moody Road.

LISBON ROUTE 196 CORRIDOR MASTER PLAN



1 GATEWAY/ COMMERCIAL AREA	2 TRANSITION AREAS	3 LISBON VILLAGE	4 CIVIC & COMMERCIAL	5 CENTER VILLAGE	6 TRANSITION AREA	7 COMMERCIAL AREA	8 GATEWAY/ FALLS VILLAGE
Vision and Character <ul style="list-style-type: none"> Commercial highway character Mixed scale, large to small development Large setbacks for large buildings, smaller setbacks for smaller buildings 	Vision and Character <ul style="list-style-type: none"> Village character along road corridor Opportunities for large-scale development off-corridor (e.g. Kelly Park, Commercial Zone) Emphasize scenic (river) and village character 	Vision and Character <ul style="list-style-type: none"> Dense, mixed-use Village scale and character Emphasize village redevelopment and infill 	Vision and Character <ul style="list-style-type: none"> Village character, village or highway scale Diversified development District should be mixed-use and not large scale 	Vision and Character <ul style="list-style-type: none"> Dense, mixed-use Village scale and character Allow for conversion from residential to non-residential, maintain the "fabric" of the village 	Vision and Character <ul style="list-style-type: none"> Both mixed-use and mixed scale Retain wooded buffer along Route 196 Highlight scenic & recreational area by Miller Park 	Vision and Character <ul style="list-style-type: none"> Medium to large scale Commercial highway character More emphasis on local/unique character Coordinate with Lisbon School facilities improvements 	Vision and Character <ul style="list-style-type: none"> Dense, mixed-use Village scale and character Redevelop waterfront, mixed use with river access and park space
Priorities <ul style="list-style-type: none"> Develop visual character standards Provide design assistance or facade programs for businesses Provide environmental constraints technical assistance Enhance gateway area 	Priorities <ul style="list-style-type: none"> Maintain overall village character along Rte 196 Develop visual character standards Address traffic safety at Moody Road Assess the need for regulatory changes 	Priorities <ul style="list-style-type: none"> Develop visual character standards Revitalize the strip west of Village Street Make streetscape enhancements and support redevelopment of former Graziano's site 	Priorities <ul style="list-style-type: none"> Develop visual character standards Connect sidewalks to Center and Lisbon Villages Further develop vision for Diversified Development District 	Priorities <ul style="list-style-type: none"> Develop visual character standards Maintain the "fabric" of the village 	Priorities <ul style="list-style-type: none"> Develop visual character standards Assess the need for regulatory changes 	Priorities <ul style="list-style-type: none"> Develop visual character standards Enhance streetscape Provide design assistance or facade program for businesses Support intermediate and long term improvements of school facilities 	Priorities <ul style="list-style-type: none"> Redevelop Worumbo and Knight-Colt properties, provide public riverfront park Develop visual character standards Streetscape & pedestrian Provide design assistance or facade program for businesses

LISBON ROUTE 196 CORRIDOR MASTER PLAN



Facade improvements



Streetscape improvements



Envisioning redevelopment



Streetscape improvements



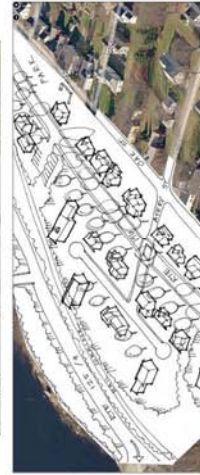
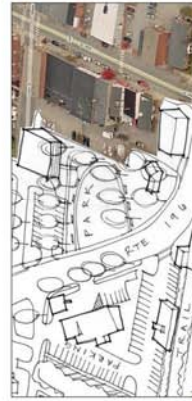
Route 196 Corridor Master Plan
 Active and Potential Future Trail Corridor
 Other Community Assets

Commercial Zone
 Village Zone
 Center or Future Public Open Space
 Potential Development Zone

Facade improvements



Facade improvements



Envisioning redevelopment



Envisioning redevelopment



Route 196 Corridor Master Plan

Introduction

Why a Master Plan?

The Route 196 corridor is the face of Lisbon. It is what most people see and experience of Lisbon, and what they associate with. Right now, the image of much of Route 196 is having a negative impact on economic and community prosperity.

The adopted Comprehensive Plan provides broad policies and recommendations on land use, transportation, and economic development, but does not have enough specific guidance on how to improve Route 196.

Establishing a vision and a master plan for the Route 196 Corridor will set the stage for future ordinance revision, policies, funding opportunities, and the development of programs and initiatives in support of revitalization along Route 196.

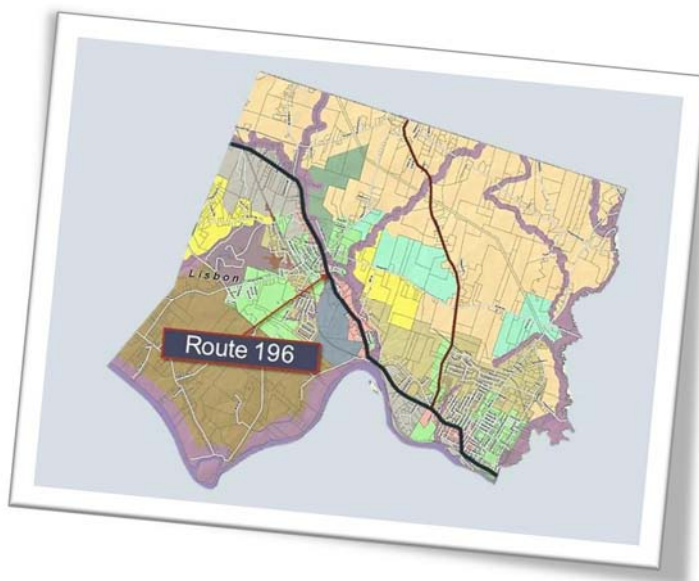
What is the “end game”?

- The Planning Board continues to seek to improve Lisbon’s zoning and ordinances, and needs community input to understand where regulatory changes may be needed.
- In these frugal times, the Town needs to be very strategic in how it spends time and money investing in the future of Route 196 – a master plan can help guide important decisions on how to use Town resources.
- A Master Plan can help attract developers and new economic opportunities by showing them what the community wants to see – given the competitive nature of the market, this is an important advantage to have.

The Reality of Prosperity and Community Development

In Maine, communities must face certain “realities” about economic markets, housing markets, local politics, and how to balance growth and community character. Ultimately, three key lessons learned come from other communities’ experience in community development:

- Visual character and aesthetics do matter
- Investment is needed to support economic development
- Quality of place is important to residents, employers, visitors and investors





Above: Visual character is an important part of the corridor's experience, and makes a place a destination rather than a pass-through – Lisbon's issues of poor visual character along this corridor limit economic and community growth.



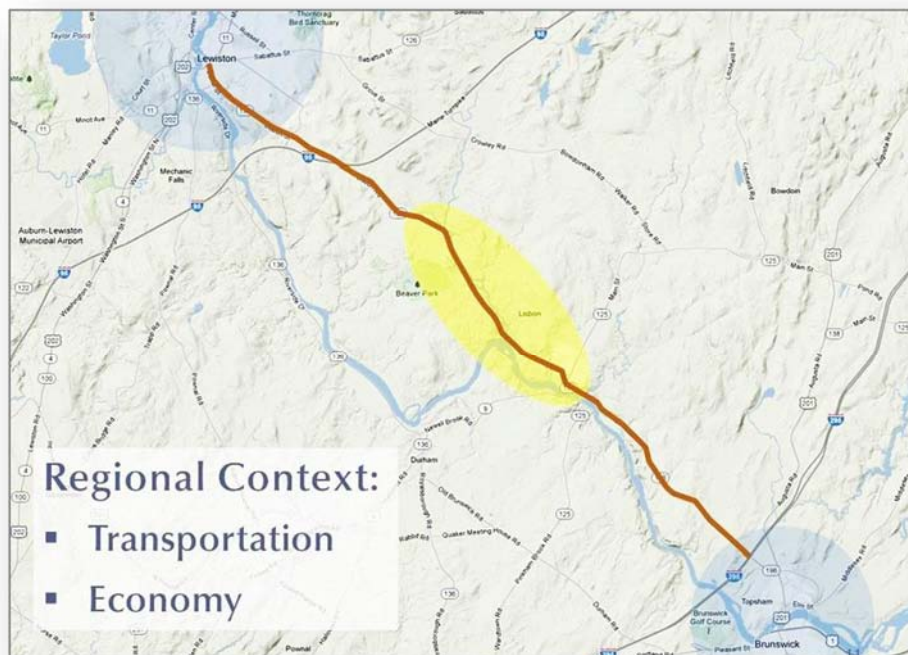
Above: Quality of place is not just about looking nice but also about having "authenticity" – Lisbon's authenticity is an asset to be capitalized on, but will require meaningful community revitalization and investment.

Regional and Local Context

The Rt. 196 corridor has very important regional significance, from a transportation and economic standpoint – this means regional demands on the corridor in addition to the town's own needs. This also means tremendous opportunities for Lisbon (e.g. high visibility by a large number of people). Regionally, Route 196 has been designated a "corridor of economic significance"; locally, Route 196 represents the spine of Lisbon's designated growth area and community core.



The Town is well positioned to see new economic development opportunities along the corridor, but has struggled with how best to enhance local character (quality of place) and make strategic public investments to best support future growth. Lisbon's section of Route 196 is about one third of the corridor between Lewiston/Auburn and Topsham/Brunswick, and also connects two major highway interchanges (I-95 and I-295).



*Excerpts from the Lisbon
Comprehensive Plan*

This planning process must be based in the policies and guidelines of the Lisbon Comprehensive Plan. This includes both policies and objectives that are specifically for the Route 196 corridor, and those that apply more broadly to land use and community development.

Land Use Policies for Route 196:

- Manage new development and redevelopment adjacent to Route 196 so that a "commercial strip" which aggravates or increases existing poor traffic flow capacities does not develop further.
- Regulate the number, spacing and size of curb cuts (entrances) along Route 196 to minimize additional traffic problems.
- Allow appropriate commercial development along the Route 196 corridor.
- That Route 196 including the two "Gateways" presents Lisbon as a progressive and attractive community.

Future Land Use Plan Principles:

- The type and density of development should be matched as closely as possible with the natural constraints of the land to absorb development. Water quality, soils, slope and the presence of unique natural features are key factors;
- The desire to maintain and enhance the "village" areas;
- The desire to continue mixed use development in Lisbon Center;
- To balance the development along Route 196 with the desire to minimize the future development of a "commercial strip" which would aggravate existing poor traffic flow capacities;
- The desire to encourage economic development including retail, commercial and industrial that is suitable for the community in appropriate areas;

(continued next page)

- The need to time public infrastructure improvements (water, sewer, roads) with development demands;
- The desire to maintain agriculture, woodland, open space and wildlife habitats;
- The desire to provide residential development at varying densities;
- The need and desire to protect ground water quality and quantity;
- The desire to maintain and enhance the natural values of the town's rivers and shorelands.

Study Area

The project area includes the entire corridor from the Topsham town line to the Lewiston town line. This includes the two main villages and the smaller Center Village, and ranges in land use from rural/undeveloped to highway commercial.



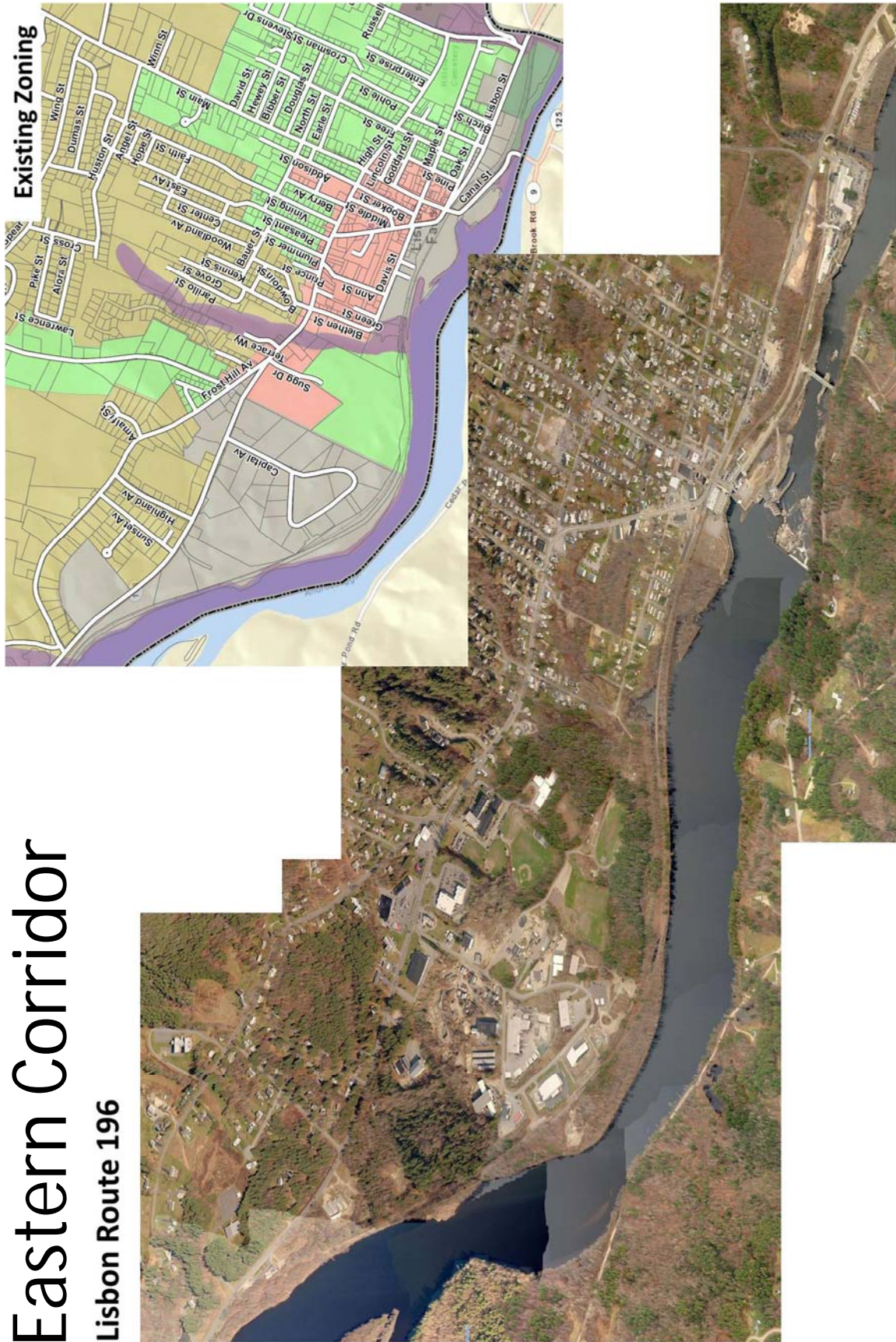
Residential versus Commercial, Public versus Private

In this Plan, the recommendations and strategies are intended to be applicable only to non-residential uses. While the *vision and planning* may be broadly inclusive of both residential and non-residential uses and structures, at this time the Plan does not include initiatives or improvements addressing residential uses along Route 196.

Many of the Plan recommendations and strategies do apply to improvements for both public and private uses and properties. For improvements or initiatives concerning private properties, the Plan indicates how the Town can influence the private sector, such as through incentives, programs, or regulatory mechanisms.

Eastern Corridor

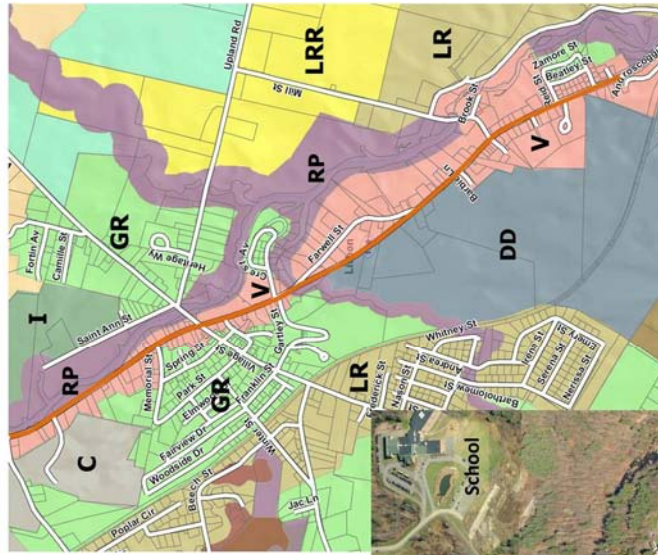
Lisbon Route 196



Central Corridor Lisbon Route 196



Wellhead Protection,
Wetlands & Steep Slopes



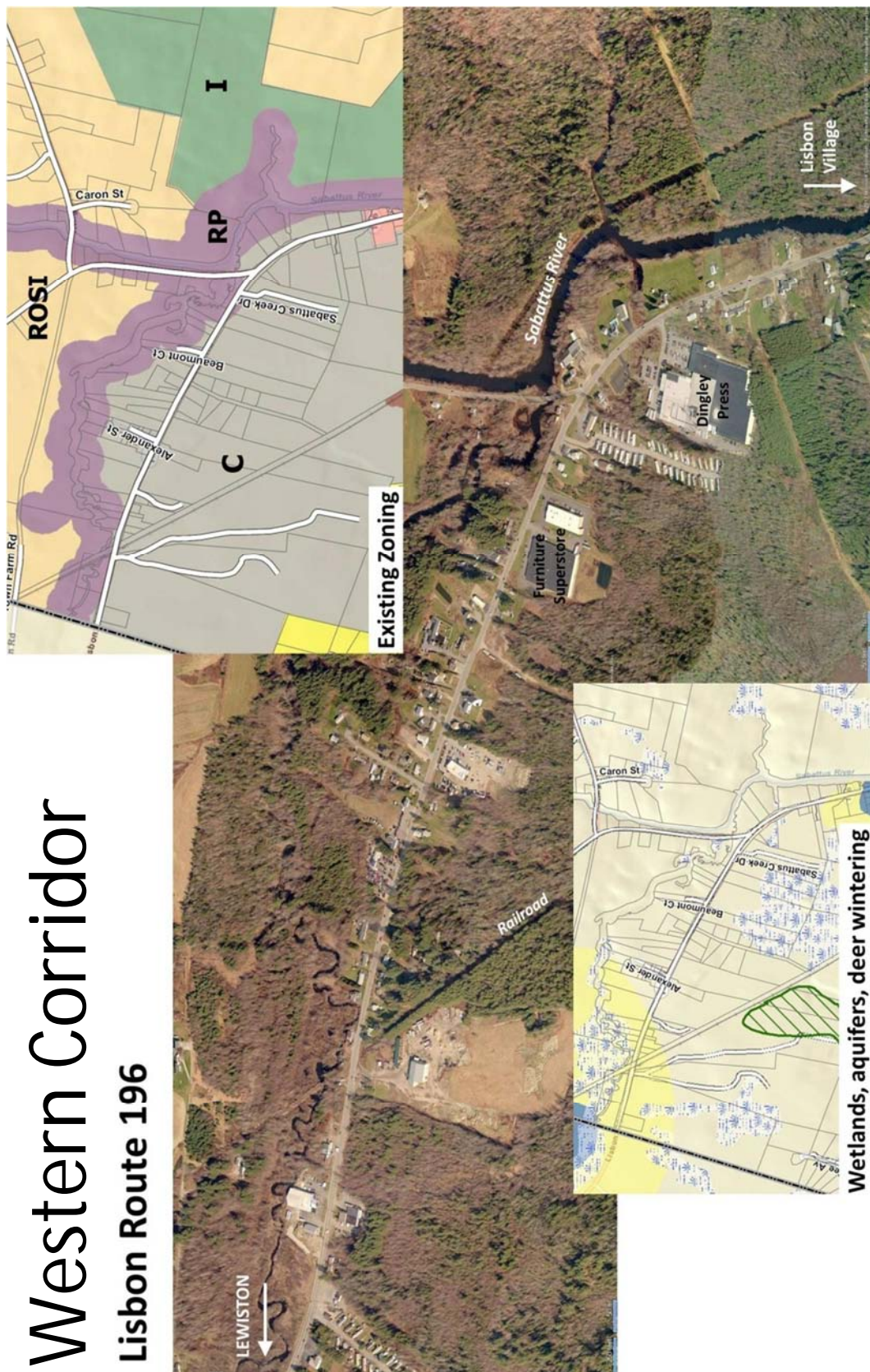
Existing Zoning



Hydric Soils

Western Corridor

Lisbon Route 196



Route 196 Eastern Corridor

Study Area

The Eastern Corridor segment is defined as the area between the Topsham town line and the Sabattus River confluence, just before Center Village.



The Eastern Corridor is described in three segments, the "**Falls Village Area**" which includes the Eastern gateway, the "**Midtown Commercial Area**" near the High School, and the "**Midtown Transition Area**" northwest of the commercial area. These areas each have different identities and character, as well as different recommendations.

Recommendations Summary

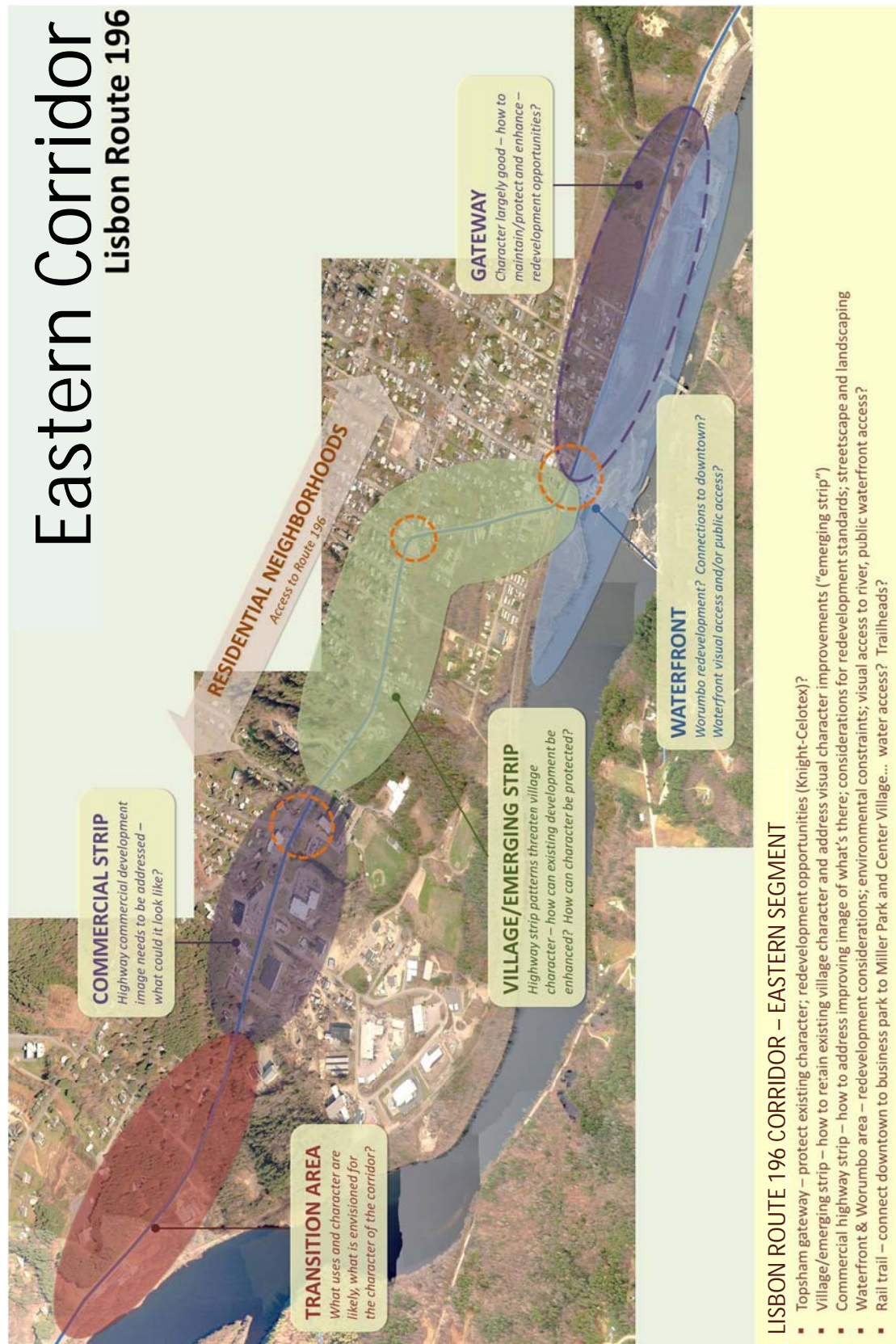
The overall objectives for the Eastern Corridor segment can be summarized as follows:

- Maintain and improve the eastern gateway character, addressing the impact of potential waterfront development on the gateway.
- Seek opportunities to develop public-private partnerships to revitalize the waterfront area, including the Worumbo Mill and Knight-Celotex sites.
- Establish standards to address visual character and aesthetics for new development and redevelopment.
- Establish programs/incentives to address visual character of existing development, such as façade or site design assistance.
- Make streetscape improvements along Route 196 through the Lisbon Falls village and the Midtown commercial area, including street trees and landscaping, decorative lampposts, and other streetscape amenities.
- Continue to improve pedestrian and bicycle conditions, such as sidewalks, crosswalks, and bike lanes.
- Pursue the creation of a rail trail between the downtown/waterfront and Sabattus River confluence, as part of the envisioned regional rail trail between Topsham and Lewiston.

(Continued next page)

- Follow up on needed long-term transportation planning, addressing traffic capacity and road design throughout the eastern corridor in a manner that compliments this plan.
- Add new and enhance existing parks and green spaces proximal to the corridor (such as on the waterfront).
- Assess the need for other regulatory changes for the “transition area” (such as setbacks and buffers along Route 196).
- Support a long-term strategy for relocating overhead utilities underground.





The Falls Village Area



Village character, density and setbacks for the Lisbon Falls village area.



The overall visual character could largely be addressed through streetscape and façade improvements.

Location

The Falls Village Area of the Eastern Corridor lies between the Topsham town line and the high school, including the eastern gateway area, the waterfront (former Worumbo and Knight-Celotex sites), and midtown commercial highway area.

Challenges

- Overall visual character and commercial development – maintaining village character and avoiding the look of a commercial strip.
- Visual character improvements to existing buildings and properties (versus new development).
- Seeking/supporting redevelopment opportunities for the waterfront (Worumbo and Knight-Celotex sites).
- Need for streetscape improvements (street trees, lampposts, sidewalks, etc.) throughout village corridor, linking to Main Street/downtown.



The village area would benefit from public and private improvements that enhance the village character and maintain the village scale.

- Need for pedestrian and bicycle improvements, such as sidewalks, crosswalks, ADA accessibility, bike lanes, and connections to the proposed rail trail.
- Short- and long-term traffic congestion and mobility issues between the Main Street/Route 125 intersection and the Route 9/high school intersection.

Vision and Character

The Falls Village Area of the corridor is envisioned as a dense, mixed-use area with a traditional village character, where village scale development should be maintained (small to medium scale). The relationship and connections to the downtown (Main Street) and the waterfront must be a part of any future public or private design or development, including the incorporation of the vision and recommendations of the *Envision Downtown* report.

Ensuring new structures conform with the scale and architecture appropriate to the village, investing in public streetscape and landscaping, working with existing building owners on building and site enhancements, and promoting reuse rather than demolition of existing residential buildings, will all be important to maintaining and improving the village character.

Future improvements and visual character in the Falls Village area must also reflect the vision in the "Envision Downtown Lisbon" plan (illustration, right), as this plan integrates the vision and recommendations from that document.





The eastern gateway, where drivers are welcomed to Lisbon, is an important area to maintain good visual character.



Example of village character applied to a façade rehabilitation for a franchise business.



Examples of streetscape amenities or public art that can create visual interest for a village corridor like the Falls Village.

Priorities

- The redevelopment of the Worumbo and Knight-Celotex sites, including potential public waterfront access and connections to the downtown and proposed rail trail.
- Develop visual character standards for new development and redevelopment.
- Invest in streetscape improvements, such as street trees, decorative lampposts, and amenities (banners, benches, etc.).
- For existing development, provide design assistance or other support for façade or site improvements.
- Establish more parks and green spaces proximal to Route 196, in particular the on the waterfront, and develop the rail trail as a recreational destination.
- Make additional pedestrian and bike improvements, such as sidewalks/ADA accessibility, safety and design of crosswalks, and bike lanes or designated routes.
- Consider establishing a Downtown TIF District to fund improvements, or incentivize façade/property improvements.

Long-Term

- Establish sidewalks on both sides of Route 196 through the village area.
- Continue to evaluate long-term options to alleviate traffic congestion and mobility issues between Main Street/Route 125 and Route 9/high school.
- Support a long-term strategy for burying or relocating overhead utility wires.

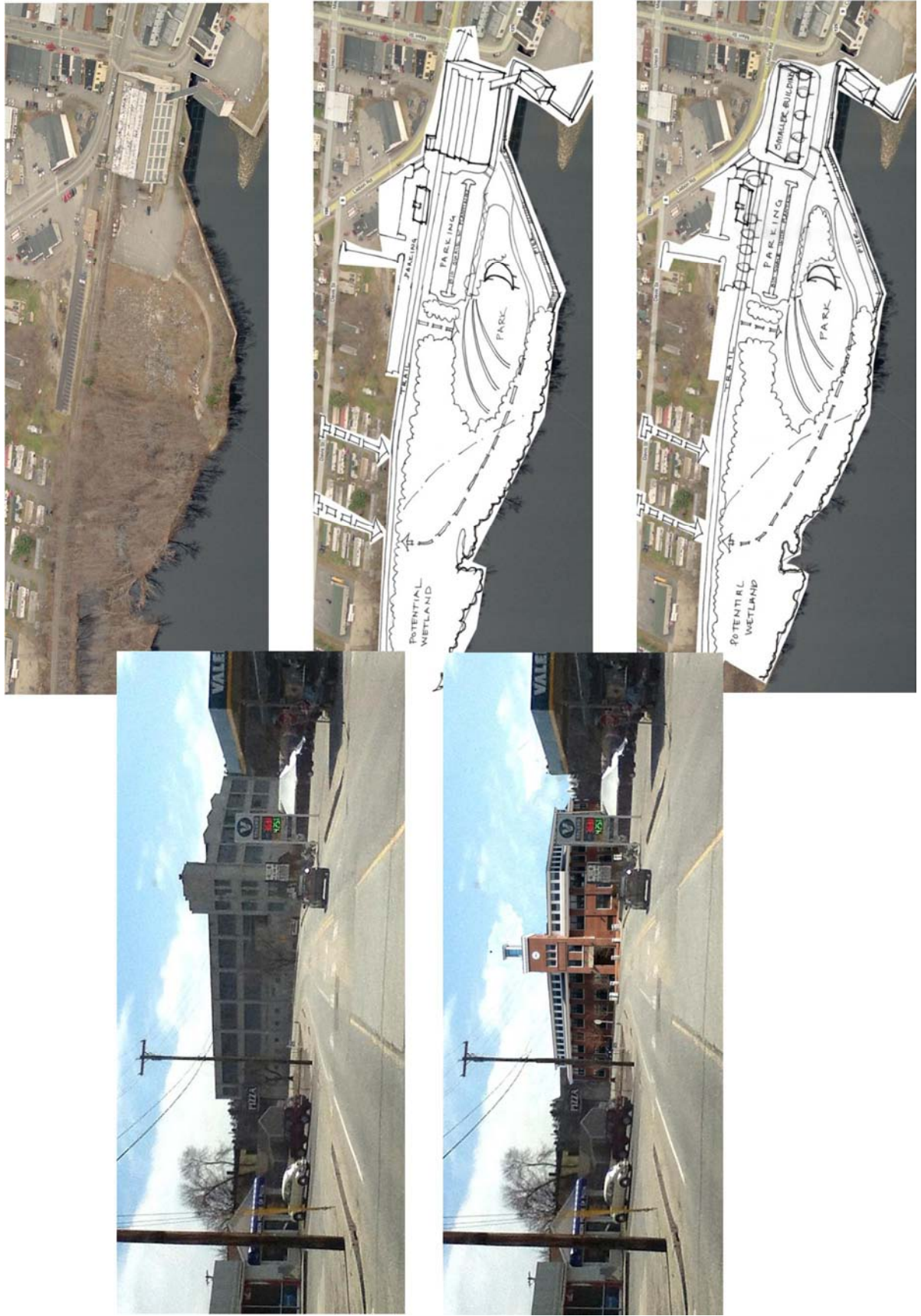
Streetscape and Façade Graphics – Before & After

Photo-simulations that approximate the visual impact of building façade and streetscape enhancements:



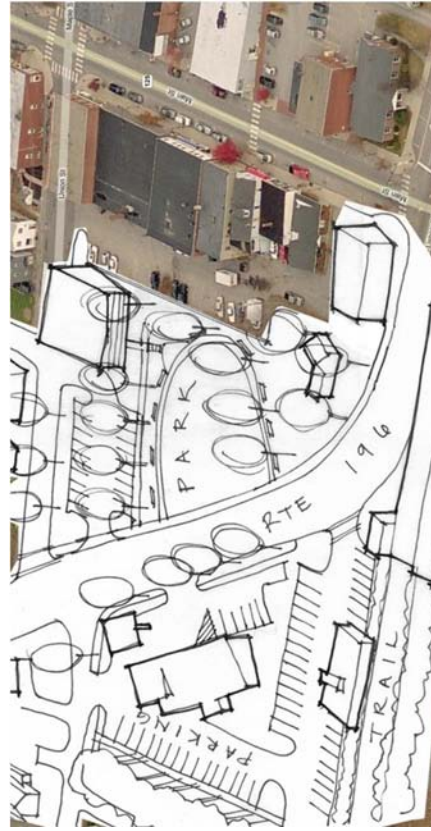
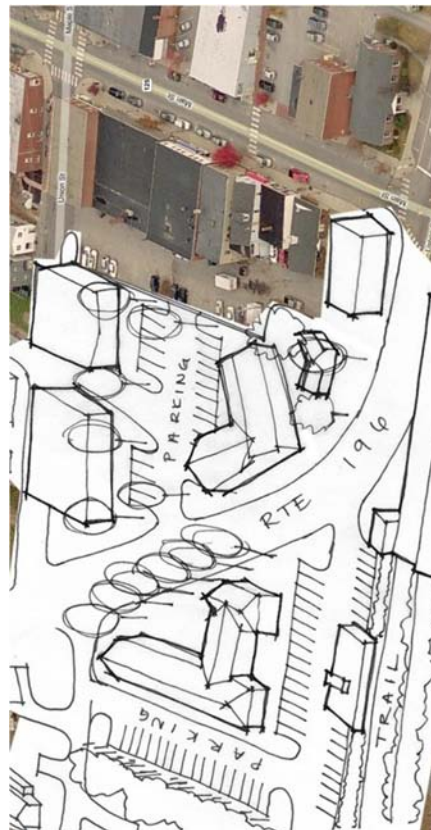
Redevelopment Concepts – Before & After

Sketches illustrating the revitalization of the Worumbo Mill site, with or without the mill:



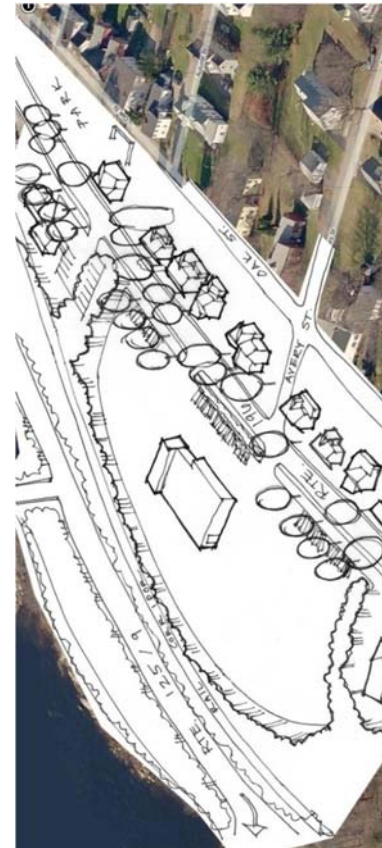
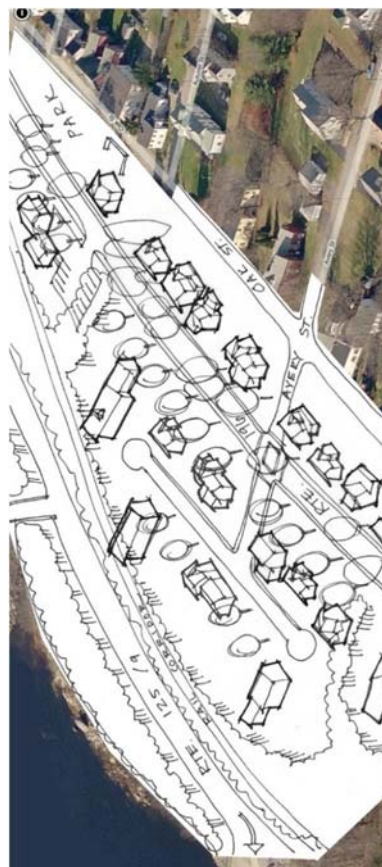
Redevelopment Concepts – Before & After

Sketches illustrating redevelopment (streetscape, private development) just around the corner from the downtown:



Redevelopment Concepts – Before & After

Sketches illustrating redevelopment concepts (streetscape, private development) near the eastern gateway:



The Midtown Commercial Area



Existing character of the midtown commercial highway strip.



Location

The Midtown Commercial Area lies between the high school and the end of the commercial strip by Subway and Aubuchon.

Challenges

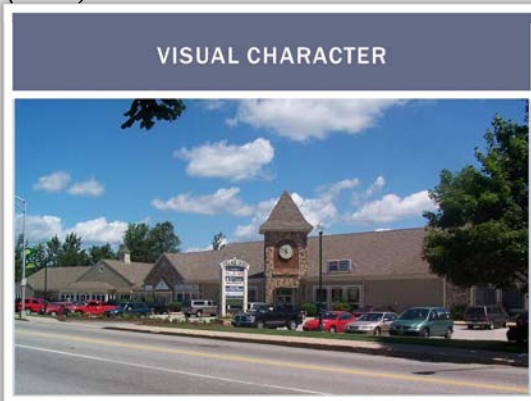
- Visual character compromised by “typical” highway building design, site layout, and signage.
- How to affect the design of existing development/owners, versus new/future development.
- Traffic congestion due to high volume of turning traffic and multiple traffic lights; traffic associated with the Route 9 intersection and the high school.
- Design challenge of how to improve the visual character through streetscape (public).
- Consideration for traffic circulation and the design development and investment in building improvements at the high school.



Aubuchon Hardware, a newer development, has good highway character, with positive building design features, landscaping, and parking to the side of the building.



Examples of highway commercial developments with ample streetscape and landscaping (above) and well-designed commercial strip buildings (below).



Vision and Character

The Midtown Commercial Area of the corridor is envisioned to continue as a location for medium to large scale commercial and retail uses, and is an area that provides important products and services to residents and passers-through. The existing character may be described as “typical commercial highway strip”, and although there is some green space and landscaping, overall the clutter and lack of design character gives off a poor image. The vision for the future character of the midtown commercial area is to work to develop streetscape and roadway design improvements, and private façade and site improvements that will improve on the commercial character by emphasizing enhancements that reflect a more unique, local character and sense of place. Addressing signage clutter and overhead utilities will also improve this corridor visually. Many of these character improvements may be long-term efforts.

Lisbon High School and school properties are also a significant anchor for the Midtown Area, at the transition between the village and commercial area. The school facilities and activities have a major impact on the Route 196 corridor, and future plans for this property should be coordinated with the Route 196 vision, including consideration for road and transportation planning.

Priorities

- Incentives and public-private partnerships with existing property owners to make façade and site improvements.
- Establish appropriate visual character standards for new development and redevelopment.
- Develop designs for public streetscape improvements, including street tree plantings and amenities (lampposts, banners, benches, etc.).

Long-Term

- Support intermediate to long-term investment in proposed high school buildings, coordinate on Rt. 196 visual character.
- Strengthen the connection to the proposed rail trail, and strengthen the pedestrian connections between surrounding residential neighborhoods and the commercial area.
- Support a long-term strategy for burying or relocating overhead utility wires.

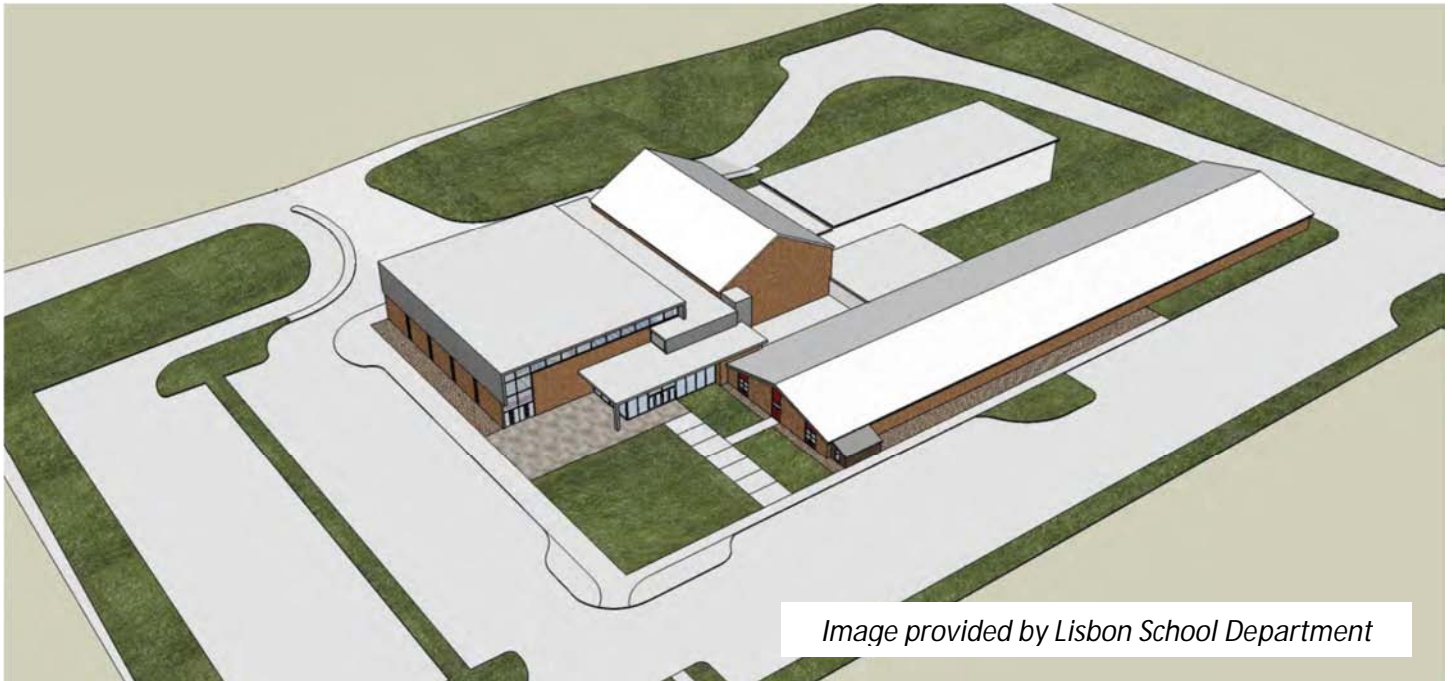
Façade & Streetscape Graphics – Before & After

Sketches illustrating concepts of façade redevelopment and streetscape enhancements in the midtown:



Lisbon School Facilities

Future plans for Lisbon High School are likely to include a new gymnasium and improved parking and traffic circulation, with the option for a performing arts space as well. The renderings below (provided by the School District) illustrate the new facilities, which are much needed physical improvements to improve the school's state ratings. The schools and their facilities are of critical importance to the community, in meeting the needs for public education, and in the bigger picture being an economic asset that affects the local work force and creates a major destination for visitors and residents. Future improvements to the school facilities should be coordinated with possible improvement projects for the Route 196 corridor, including streetscape and road improvements.



The Midtown Transition Area



Existing character of the midtown transition area.



Location

The Midtown Transition Area lies between the end of the midtown commercial area (Subway/Aubuchon) and the Sabattus River confluence (Miller Park/boat launch). This is a largely undeveloped stretch of road characterized by wooded buffers and also higher traffic speeds.

Challenges

- Avoiding the spread of the “commercial strip” into this area.
- Allowing that this area has commercial growth potential, while acknowledging the significance this wooded portion of the corridor plays in the overall experience of Route 196.
- Natural resource constraints associated with the river (land area between the road and the river narrows in this area).
- Traffic speeds and maintaining safety (especially for turning traffic) as growth occurs.



This transition area ends at Miller Park, where the river boat launch and Paper Mill Trail are accessed.



There are also good views to the Androscoggin River (above) and Sabattus River, at the confluence.

Vision and Character

The Midtown Transition Area of the southern corridor, like the transition area of the northern corridor, serves as both mixed use and mixed scale (both highway commercial and village residential). The overall character of this transition area should remain wooded, allowing for development along the corridor to access Route 196, but generally retain a wooded buffer and larger setbacks along the road. The scenic and recreational area at Miller Park (the boat launch and trail head) should also be acknowledged as an important feature of the corridor and valued community resource.

On the river side of the road, the scale of future commercial development will likely be dictated by site constraints, but the uses that do not rely on road exposure and frontage (such as retail) would likely not fit with the desired character of the corridor. On the other side, future development will also likely be restricted somewhat by site constraints (steep slopes along the frontage), and any extension of the adjacent residential neighborhood would likely benefit from separation and buffering from Route 196.

Priorities

- Maintaining and enhancing existing wooded character.
- Assess the need for regulatory changes, such as increased setbacks or buffer/landscape requirements for properties along Route 196.



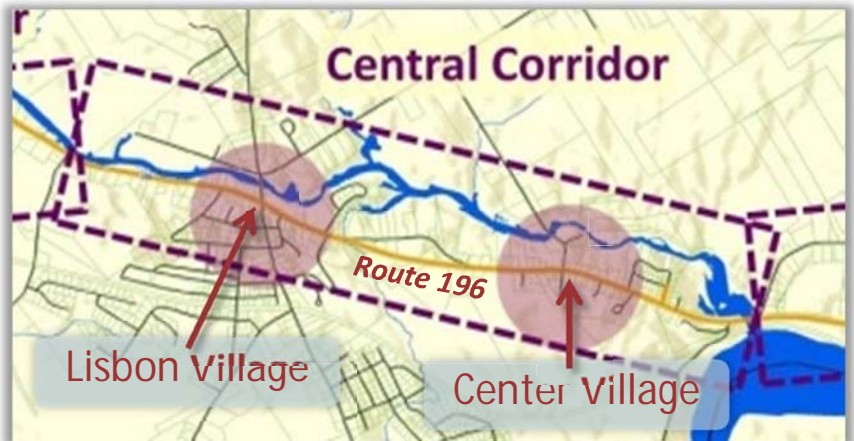
Long-Term

- Improve pedestrian and bike connectivity through this section, such as bike lanes/route and connections to the proposed rail trail.

Route 196 Central Corridor

Study Area

The Central Corridor is defined as the area between Moody Road near Kelly Park and the Sabattus River confluence by Miller Park and boat launch.



The Central Corridor is described in four sections, the "Center Village Area"; the "Civic & Highway Area" including the post office, credit union and town office; the "Lisbon Village Area"; and the "Lisbon Village Transition Area" including Kelly Park, which is related to the Transition Area described in the Northern Corridor. These areas each have different identities and character, as well as different priorities and recommendations. See corridor segment map, following the Recommendations Summary below.

Recommendations Summary

The overall objectives for the Central Corridor can be summarized as follows:

- Establish standards to address visual character and aesthetics for new development and redevelopment.
- Establish programs/incentives to address visual character of existing development, such as façade or site design assistance.
- Maintain the "fabric" of the village in the Center Village, but allow for conversion from residential to non-residential or mixed uses.
- Support opportunities for residential growth off Mill Street.
- Take a more active role in visioning and planning for future growth in the Diversified Development Zone, its relationship to Route 196 and the village.
- Work with developers of the former Graziano's site to build positive village character at the Village Street intersection; invest in public improvements (streetscape, pedestrian) in this area.

(Continued next page)

- Extend street trees and decorative lampposts along Route 196 north of Village Street (to MT Pools or Kelly Park).
- Seek opportunities to add new parks and green spaces proximal to the corridor; enhance the Flea Market area with more of a “public market feel”.
- Evaluate options to improve safety and traffic speeds at the Moody Road intersection.
- Continue to improve pedestrian and bicycle conditions, such as sidewalks, crosswalks, bike

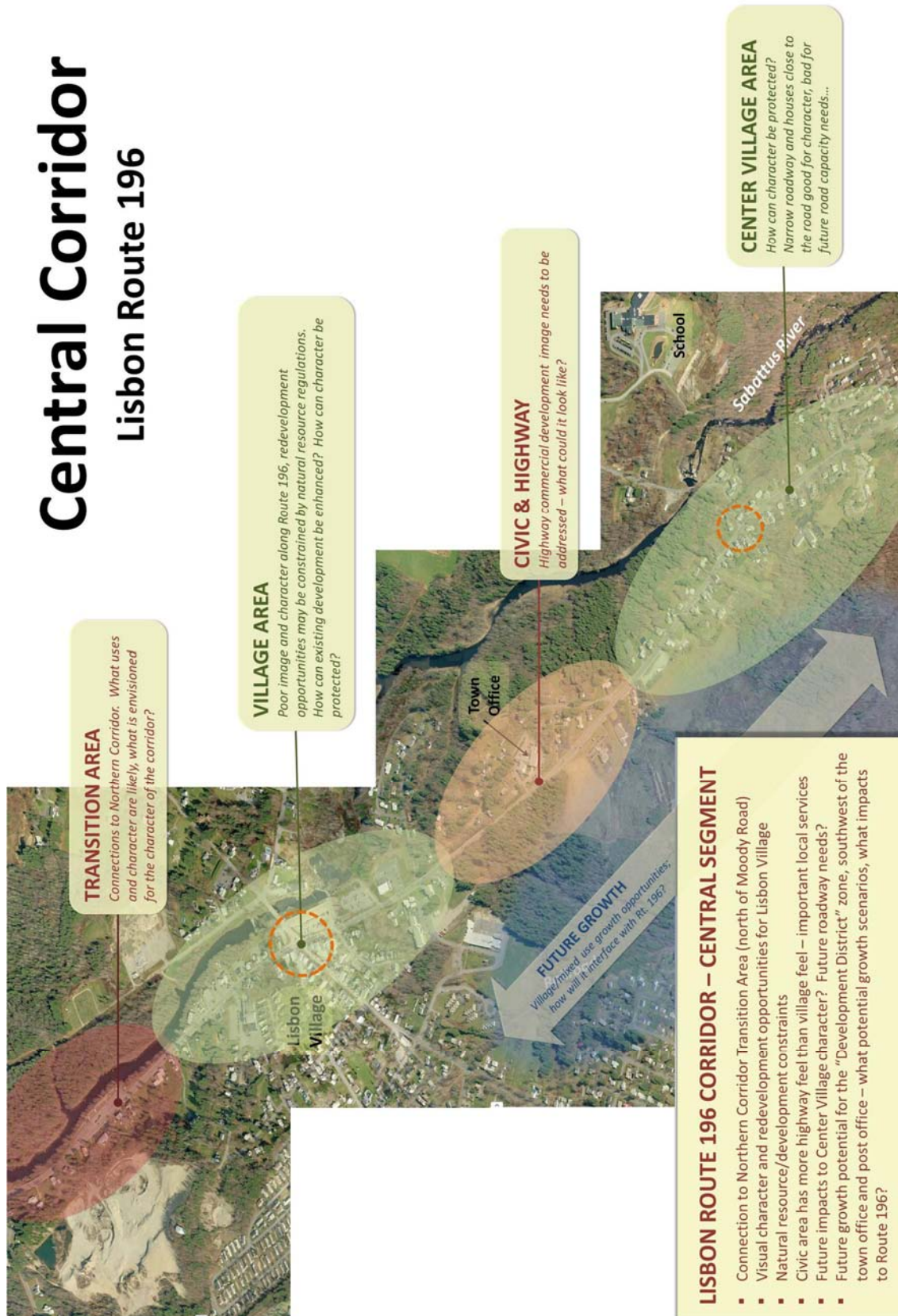
lanes, and proposed future rail trail connections.

- Assess the need for regulatory changes (land use and development) for the “transition area”; align the vision and land use regulations to the transition area identified in the Northern Corridor.
- Support a long-term strategy for relocating overhead utilities underground (expensive but visually significant).



Central Corridor

Lisbon Route 196



The Center Village Area



Typical character, density and setbacks for the Center Village.



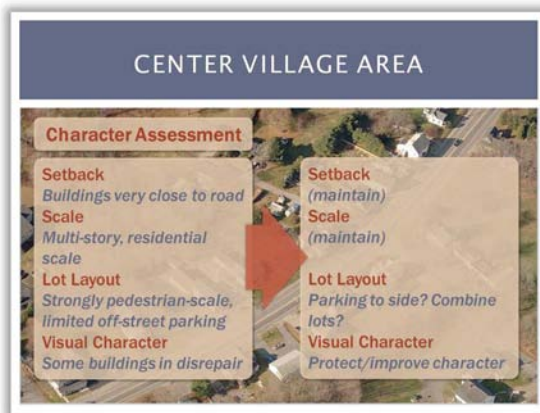
Future traffic capacity and future development surrounding Center Village will have impacts on the road design and cross-section.

Location

The Center Village Area lies between Miller Park/the Sabattus River confluence and the post office, and includes the Town Office and the Mill Street connection to the Lisbon Community School. It backs onto the Sabattus River to the northeast, and onto the Diversified Development Zone to the southwest.

Challenges

- Maintaining the “fabric” of the village as houses convert from residential to non-residential or mixed use.
- Potential impacts of off-corridor development in the Diversified Development Zone and Mill Street area.
- Managing future driveway and parking access needs (given many small lots) as land use change occurs.
- Limited road right-of-way and buildings close to the road may be a future concern if road widening or turn lanes are needed.
- Need for pedestrian and bicycle improvements: sidewalks should be on both sides, crosswalks such as at Mill Street, ADA accessibility, and bike lanes.



The existing and desired character is described in terms of building setback, scale (size), and the layout of development on a lot, as well as the visual and aesthetic qualities.



Center Village near the Mill Street intersection.



Many of the structures in Center Village are still residential, but some include small businesses in converted houses. More of these types of conversions can be expected along Rt. 196.

Vision and Character

The Center Village Area of the corridor is envisioned as a small but dense, mixed-use area, where village scale and traditional village character should be maintained (small to medium scale). Though historically this village has been predominantly residential, it is anticipated that it will continue to trend towards small businesses and rental and/or multi-family units. The development of and connections to the Mill Street area (Lisbon Community School) and the Diversified Development Zone may have future impacts on the village and roadway.

As change occurs in the Center Village, it will be important to maintain the “fabric” of the village in terms of scale, setback, and maintaining historic homes. It is recognized that many of the homes and structures in the village are in poor condition or not well maintained, which should be addressed before buildings are lost.

Priorities

- Develop visual character standards for new development and redevelopment.
- Maintain the “fabric” of the village as uses and ownership changes, encouraging the maintenance/restoration of existing buildings and appropriate scale and design for new buildings.

Long-Term

- Establish sidewalks on both sides of Route 196 through the village area, and develop a pedestrian connection to Mill Street, the existing bike-pedestrian trail, and the Lisbon Community School.
- Monitor access management and future need to address road widening, turning lanes, or other road improvements as growth occurs in this area.

The Civic & Highway Area



Looking north at the Town Office.



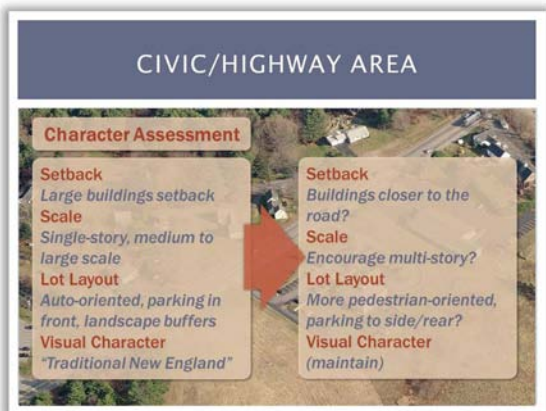
Across from the Town Office is a potential growth area, which could represent an expansion of Lisbon Village.

Location

The Civic & Highway Area of the Central Corridor lies between the post office and the outer edge of Lisbon Village (by Napa), and includes the Lisbon Town Office. It includes the section of the Diversified Development Zone (future growth area) that abuts directly onto Route 196.

Challenges

- Represents a distinct change in character between the Center Village and Lisbon Village – wider road and building setbacks.
- Potential impacts of abutting growth area (Diversified Development Zone).
- No pedestrian or bicycle infrastructure between villages.



A range of building scales and setbacks may work in this section between the villages – maintaining village character is important to the visual character.



There is a good opportunity to develop more of a vision for the potential growth area and expansion of Lisbon Village behind the post office and credit union.

Vision and Character

The Civic & Highway Area of the corridor is envisioned as a transition between the two villages, where either village scale or highway scale could be appropriate for future development, so long as traditional New England character is maintained. Larger setbacks consistent with the setbacks of the post office, town office, and credit union are appropriate if the frontage is landscaped and well maintained.

The envisioned future growth of the Diversified Development Zone will have direct impacts on this stretch of the corridor, and there is strong interest to develop the vision for this zone further. Large footprint buildings (Dingley Press or Furniture Superstore scale) are not envisioned for this area; small to medium scale buildings, possibly building on the Lisbon Village development pattern, is appropriate.

Priorities

- Develop visual character standards for new development and redevelopment.
- Extend a sidewalk to connect between villages.
- Further develop the vision for the Diversified Development Zone, consider pro-active planning and development measures.



Long-Term

- Extend village streetscape from the Farwell Mill to the post office.
- Consider potential aesthetic and safety improvements for the roadway.

Civic & Highway Area Development Scenarios

The design and layout for new development or redevelopment in this section could follow one of the scenarios (highway scale or village scale):

EXISTING CORRIDOR



An existing site with future growth potential.

DEVELOPMENT SCENARIO - 1



Village Scale. This photo-simulation emphasizes a small setback and village scale building.

DEVELOPMENT SCENARIO - 2



Highway Scale. This scenario shows what a larger scale building such as a small strip mall could look like. Such developments should emphasize good building design and ample landscaping, especially if parking is in front.

DEVELOPMENT SCENARIO - 3



Village Scale 2. Housing or mixed use could work for this corridor, this scenario again emphasizes small setbacks and village scale buildings.

The Lisbon Village Area



The Farwell Mill is a landmark building in Lisbon Village.



Just north of the village center, the Route 196 corridor's visual character is most in need of improvement.

Location

The Lisbon Village Area of the Central Corridor lies between Napa Auto east of the village and M-T Pools west of the village (before Kelly Park), and includes the Village Street intersection also known as Graziano's Square. The Sabattus River lies just to the northeast, and the bulk of Lisbon Village lies just to the southwest.

Challenges

- Maintaining village character along Route 196 – particularly with significant changes anticipated with the redevelopment of the former Graziano's site at the corner of Village Street and Route 196.
- Need for streetscape improvements (street trees, lampposts, sidewalks, etc.) – extending streetscape along Route 196 throughout this section.
- Revitalizing/enhancing the Route 196 strip north of the Village Street intersection – visual character improvements to buildings and properties, fill vacancies. (*Continued next page*)



View of the former Graziano's site, in the center of the village.

- Overlapping natural resource constraints and regulations (including shoreland zoning and aquifer protection overlay zone) may pose some redevelopment challenges for certain properties.
- Determining whether on-street parking on Route 196 near the Graziano's site is more important to keep to serve businesses, or better to eliminate to provide better streetscape and reduce traffic and bicycle safety issues associated with parallel parking.
- Need for pedestrian and bicycle improvements, such as sidewalk repair, crosswalk improvements, ADA accessibility, bike lanes.

Vision and Character



The village character should be maintained through setbacks, lot layout, and building design.

The Lisbon Village Area of the corridor is envisioned as a dense, mixed-use area, where village scale and traditional village character should be maintained (small to medium scale). The relationship and connections to Village Street and the portion of Lisbon Village off Route 196 must be taken into consideration with the planning of public or private improvements and redevelopment in this area.

Portions of Route 196 both east and west of the Village Street intersection currently suffer from a loss of village character. Ensuring new structures conform with the scale and architecture appropriate to the village, investing in public streetscape and landscaping, working with existing building owners on building and site enhancements, and promoting reuse rather than demolition of existing residential buildings, will all be important to maintaining and improving the village character.



Signage, landscaping, and streetscape are important elements of Lisbon Village's character.

Priorities

- Develop visual character standards for new development and redevelopment.
- Work to revitalize/enhance the strip west of the Village Street intersection – façades, redevelopment, and streetscape.
- Make sidewalk and streetscape enhancements (lighting, etc.) to compliment anticipated new development at Graziano's Square, and to support revitalization west of Village Street.

Long-Term

- Improve sidewalks on both sides of Route 196 through the village area, and develop connections to alternative pedestrian routes that parallel Route 196, including access to the envisioned rail trail.
- Support a long-term strategy for burying or relocating overhead utility wires.

This mapping and 3D modeling illustrates the resource protection areas, zoning, and general village layout.



Lisbon Village Streetscape & Buildings

Streetscape and shoulder improvements north of Lisbon Village are important to improving image and character:



Top: existing streetscape north of Village Street

Below: visualizations showing streetscape with grass esplanades and new sidewalks, left, and with mature street trees, right.



Building massing and scale is important to the design of new development or redevelopment in the village:



Top: the building massing of the former Graziano's

Below: a new building on that site not in scale, left (large box, no façade articulation), and one with more village scale and basic village architectural form, right, even just in basic building massing.



Lisbon Village Transition Area



The transition area includes a mix of residential and commercial uses.

Location

The Lisbon Village Transition Area lies between M-T Pools and Moody Road. It includes the recently approved Kelly Park development to the southwest, and is very closely paralleled to the northeast by the Sabattus River (some areas with minimal frontage between the road and the river). This Transition Area abuts the Western Transition Area identified in the Western Corridor segment, and has many of the same issues and opportunities.



The transition area also includes a stretch right next to the Sabattus River.
Central Corridor

Challenges

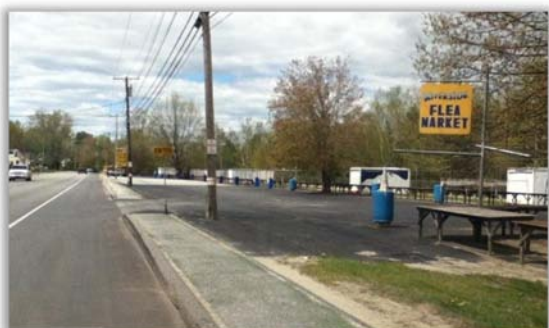
- Determining what development scale and character is most appropriate, as this area (and the Western Transition Area) support mixed scales and character.
- Maintaining (and enhancing) existing character: residential properties, well-landscaped businesses, views to the river.
- Traffic speeds and maintaining safety, especially for turning traffic at Moody Road and as growth occurs at Kelly Park.



The character and scale of the Lisbon Village Transition Area should be similar to the adjacent Northern Transition Area.



Traffic speeds and turning traffic safety at the Moody Road intersection is a significant concern.



The flea market area could be enhanced with streetscaping and better pedestrian access.

Vision and Character

The Transition Area of the Central Corridor is envisioned as an extension of Lisbon Village, though may not be appropriate for the same density along Route 196 due to access management/traffic management constraints. This area, like the Western Transition Area, is envisioned as both mixed use and mixed scale (both highway commercial and village scale). Along the Route 196 frontage, development should remain smaller in scale, and maintain existing setbacks and scenic and village character. For land behind this frontage, such as Kelly Park, the scale of development may be larger to accommodate commercial growth. The relationship between Kelly Park and Lisbon Village will be important consideration as individual sites in the park are developed.

The village streetscape elements, including sidewalks and lampposts, should be extended up to Kelly Park and the Flea Market, and the opportunity may exist to make visual enhancements to the Flea Market. The Town should work with existing landowners to encourage façade and landscaping improvements to enhance the village character in the Transition Area.

Priorities

- Maintain village character and scale for the Route 196 frontage, and seek to maintain consistent character between this Transition Area and the Western Transition Area (Moody Road to Littlefield Road).
- Establish visual character standards for new development and redevelopment.
- The Moody Road intersection is currently seen as a dangerous area for traffic due to high speeds, poor visibility for traffic turning onto Route 196, and poor visibility of left turning traffic onto Moody Road – safety improvement to this intersection must be addressed.

Long-Term

- Improve pedestrian and bike connectivity through this section.
- Maintain river views and consider opportunities for future public river access or a small park.

Route 196 Western Corridor

Study Area

The Western Corridor is defined as the area between the Lewiston town line and Moody Road, just before Lisbon Village.

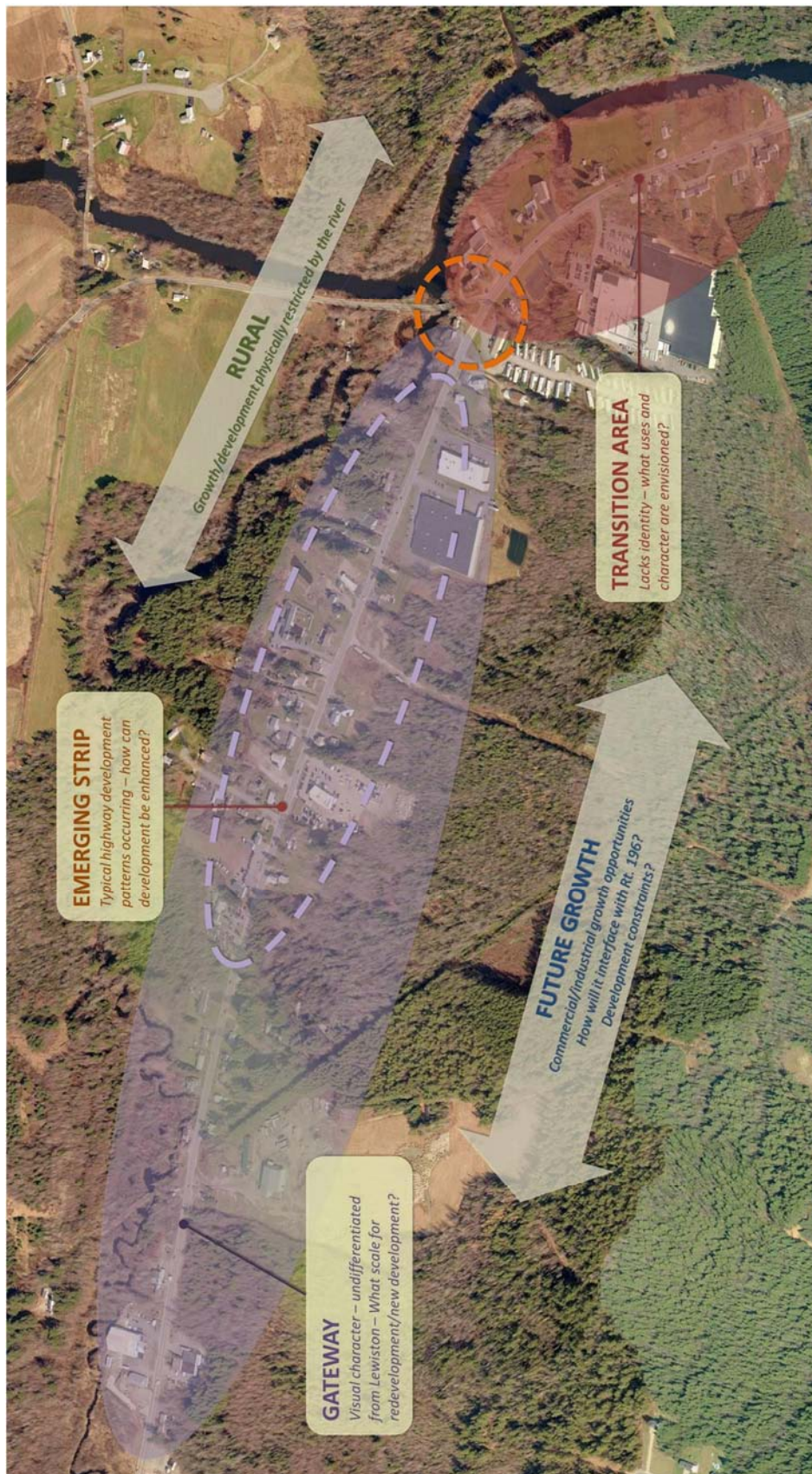


The Western Corridor is described in two parts, the “**Western Transition Area**” and the “**Western Gateway Area**”. These two areas each have different identities and character, as well as different recommendations. See *corridor segment map*, next page.

Recommendations Summary

The overall objectives for the Western Corridor can be summarized as follows:

- Improve the western gateway area.
- Establish standards to address visual character and aesthetics for new development and redevelopment.
- Establish programs/incentives to address visual character of existing development, such as a design assistance program.
- Assess the need for regulatory changes (land use and development), such as rezoning the “transition area” or considering changes to allowed land uses or dimensional standards.
- Ensure traffic mobility and safety continue to be addressed through standards and alternative traffic design, avoiding the need for traffic lights and adding lanes (road widening).
- Evaluate options to improve safety and traffic speeds at the Moody Road intersection.
- Retain the scenic and historic/cultural character for the “transition area” between Littlefield Road and Moody Road.
- Support long-term improvements such as sidewalks and streetscape, trail connections and improved bicycle infrastructure, and burying or relocating overhead utilities.



LISBON ROUTE 196 CORRIDOR – WESTERN SEGMENT

- Lewiston gateway – enhancements, redevelopment?
- Visual character of highway commercial development
- Natural resource/development constraints
- Maintain residential uses? Future residential mix?
- Scenic/natural resources, access

The Western Transition Area



Looking north from Moody Road, where the road begins to widen and speeds increase.



The mix of uses (residential and commercial), as well as scales (the large-scale Dingley Press in the upper right)

Location

The Transition Area lies between the Littlefield Road intersection and Moody Road intersection, and is strongly related to the abutting Lisbon Village Transition Area. It includes a wide range of development scales and land uses.

Challenges

- Accommodating a mix of land uses and development scales, while maintaining appropriate character.
- Maintaining (and enhancing) existing character: historic homes, well-landscaped businesses, views to the river.
- Traffic speeds and maintaining safety (especially for turning traffic) as growth occurs.
- Development constraints on northeast side of the corridor (river side).

Vision and Character

The Transition Area of the Western Corridor is not strictly a commercial district, and is envisioned as both mixed use and



The existing character is somewhat mixed, but can generally be described as village character (except Dingley Press). Smaller scale will be expected along the river side of the corridor, while larger scale may be feasible on the other side.



Example of existing village scale and character.



Like Dingley Press, large-scale development should be set back from the road with well landscaped frontage.

mixed scale (both highway commercial and village scale). On the river side of the road (northeast), development should remain smaller in scale and maintain existing scenic and historic/cultural characteristics, including maintaining historic buildings. On the other side, where there is an existing mix of commercial scales, it is important to retain the village character through building design and the layout of buildings and landscaping/ streetscape. Setbacks should generally be larger for large-scale buildings, but smaller scale buildings should be closer to the road, consistent with existing setbacks.

Priorities

- Maintaining and enhancing existing character: historic homes, well-landscaped businesses, views to the river.
- Establish visual character standards for new development and redevelopment.
- Assessing the need for other regulatory changes (land use and development) such as rezoning, to reflect the vision of mixed use and mixed scale development, compatible with village character.

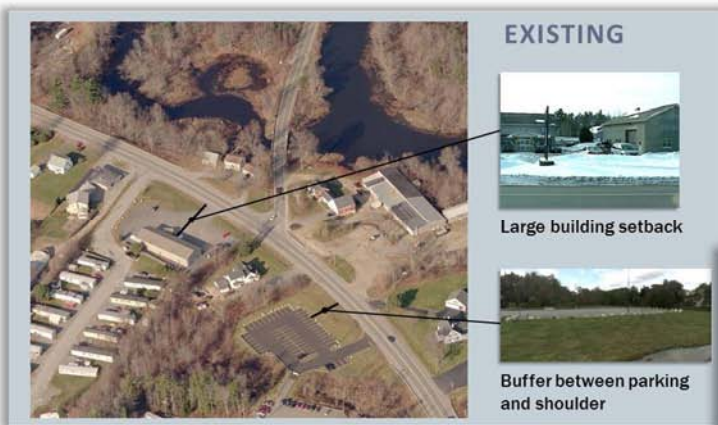


Long-Term

- Improve pedestrian and bike connectivity through this section.
- Support a long-term strategy for burying or relocating overhead utility wires.
- Maintain river views and consider opportunities for future public access or park space.

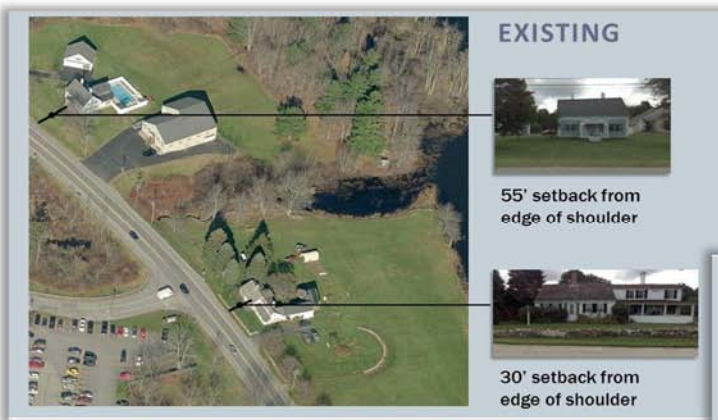
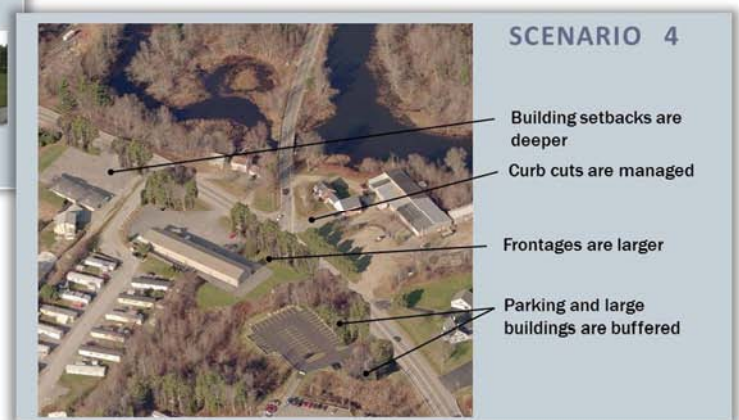
Transition Area Development Scenarios

The design and layout for new development or redevelopment in the western transition area should follow one of two scenarios:



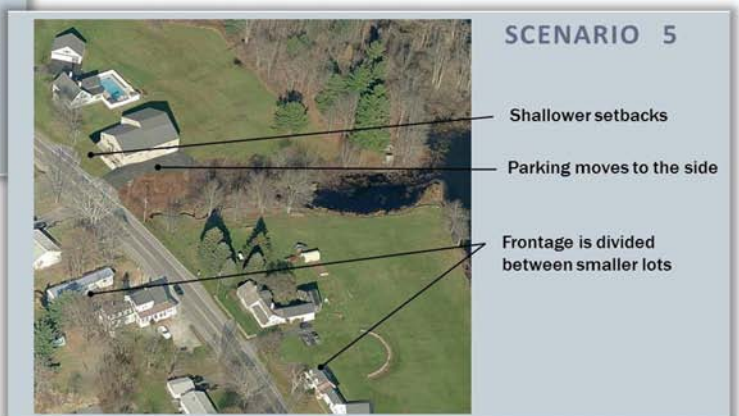
Highway Scale. Large-scale buildings should be located further from the road, with a wooded or park-like buffer and business sign at the entrance.

The photo-simulation to the right shows building expansion or redevelopment and improved streetscape and buffering between the roadway and buildings or parking.



Village Scale. Smaller-scale buildings should be located closer to the road. Good building design and landscaping will be important to the visual character for the gateway.

The photo-simulation to the right shows redevelopment and new development with smaller, village-scale setbacks for buildings, with parking to the side and rear.



The Western Gateway Area



Development at the Lewiston town line



Existing highway commercial uses such as Davis Landscape and Benoit's

Location

The Western Gateway Area includes the area between the Lewiston line and Littlefield Road intersection, paralleling the Sabattus River. This corridor segment is characterized by a wider roadway and higher speeds (50mph+).

Challenges

- Seeking commercial opportunities while avoiding the look of a commercial highway strip.
- Visual character improvements to existing buildings and properties (versus new development).
- Limited development opportunities on the northeast side of the corridor (river side); some environmental constraints on southwest side of the corridor.
- Traffic speeds and maintaining safety (especially for turning traffic) as growth occurs.
- The need for character improvements and redevelopment at the gateway (Lewiston town line).



Typical highway commercial strip uses can still contribute positively to character, with good design standards.



This car wash employs good building design, signage, and landscaping, which gives it good highway character.



Example of a large landscape buffer in front of a large-scale commercial building. The business sign is located at the driveway entrance.

Vision and Character

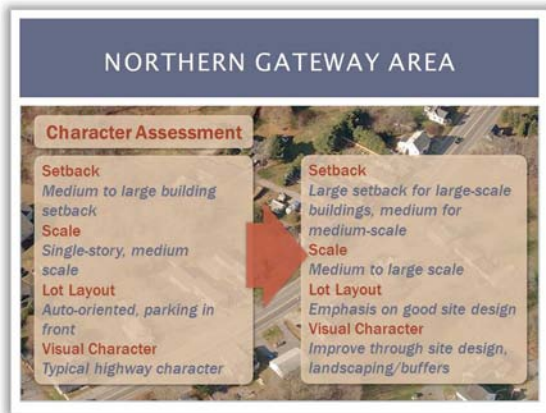
The Gateway Area of the western corridor, between the Lewiston line and Littlefield Road, is envisioned as a primarily commercial area, and would be appropriate for a mix of large and medium scale development, appropriately sited.

On the northeast side (river side) of the road, development will be limited to land directly adjacent to Route 196 due to natural resource constraints, while on the opposite side there are significant "rear lot" development opportunities to the southwest (Commercial Zone), with the possibility for business park style developments with access roads off Route 196. This commercial area has been identified as having potential environmental constraints, and though not determined to be an obstacle to development, this will be a significant factor in property layout and permitting.

Along this part of the corridor, large-scale commercial buildings should generally be encouraged to have a large setback and planted or park-like buffer along Route 196; business signs should be the primary road exposure for this scale of development. Small to medium scale development can be located closer to the road; the aesthetics of building character and lot layout for such developments will be more important, and buildings should be of a traditional New England style. Throughout this section of the corridor, opportunities to provide road improvements such as planted center medians or bicycle lanes, as well as creating collector roads to limit driveway entrances, is strongly encouraged.

Priorities

- Developing visual character standards for new development and redevelopment.
- Programs and incentives to encourage existing businesses and properties to make improvements – possibly through a design assistance program.
- For new development or redevelopment, providing assistance for further assessment of environmental site constraints (physical constraints and local & state regulatory parameters).



Long-Term

- Extend sidewalks and provide pedestrian connectivity to this section; anticipate the proposed rail-trail access and connections (e.g. potential for a trail head).
- Seek funding to support infrastructure improvements to support economic development (such as utilities, access roads).
- Consider pro-active planning and development measures for the Commercial Zone and gateway area, including roadway and streetscape.
- Support a long-term strategy for burying or relocating overhead utility wires.



(Before)



SCENARIO 3

Building design, façade

Signage

Streetscape

Parking, pavement

(After) Photo-simulation graphic showing example design improvements to an existing building and site.



Photo-simulation showing example a small-scale building, smaller building setback, appropriate building, sign, and landscape design.



Photo-simulation showing example a large, wooded setback for large-scale building, appropriate sign design.

Gateway Area Development Scenarios

The design and layout for new development in the western gateway section should follow one of two scenarios:



SCENARIO 1

Large setback, wooded

Phase in access road
for future development

Large scale building
(not visible from road)

Large-scale buildings should be located further from the road, with a wooded or park-like buffer and business sign at the entrance.

For retail uses that rely on visual access to the road, setbacks should still be maximized, with frontage that maximizes parking and has ample green space and landscaping.



SCENARIO 2

Medium setback,
smaller building

Street-oriented

Parking to side/rear,
or landscaped

Protect rear lot access

Smaller-scale buildings should be located closer to the road, though still a larger setback than in the villages. Good building design and landscaping will be important to the visual character for the gateway.

Maintaining opportunities for future rear-lot access and development, such as through road easements, is strongly encouraged.



